

## Briefing Note for Calne LHFIG

Area Board Issue 3-22-9 – A3102 Mile Elm – Request for Safety Measures.

### Background

Mile Elm is a hamlet consisting of Farms and private dwellings and is situated approximately ½ mile from the developed area of Calne, alongside the A3102. There is a 50 mph speed limit imposed but there is no system of street lighting. The main area of concern features an S-bend which should contain speeds below the posted speed limit, but collisions resulting in personal injury and “damage only” have been reported here.

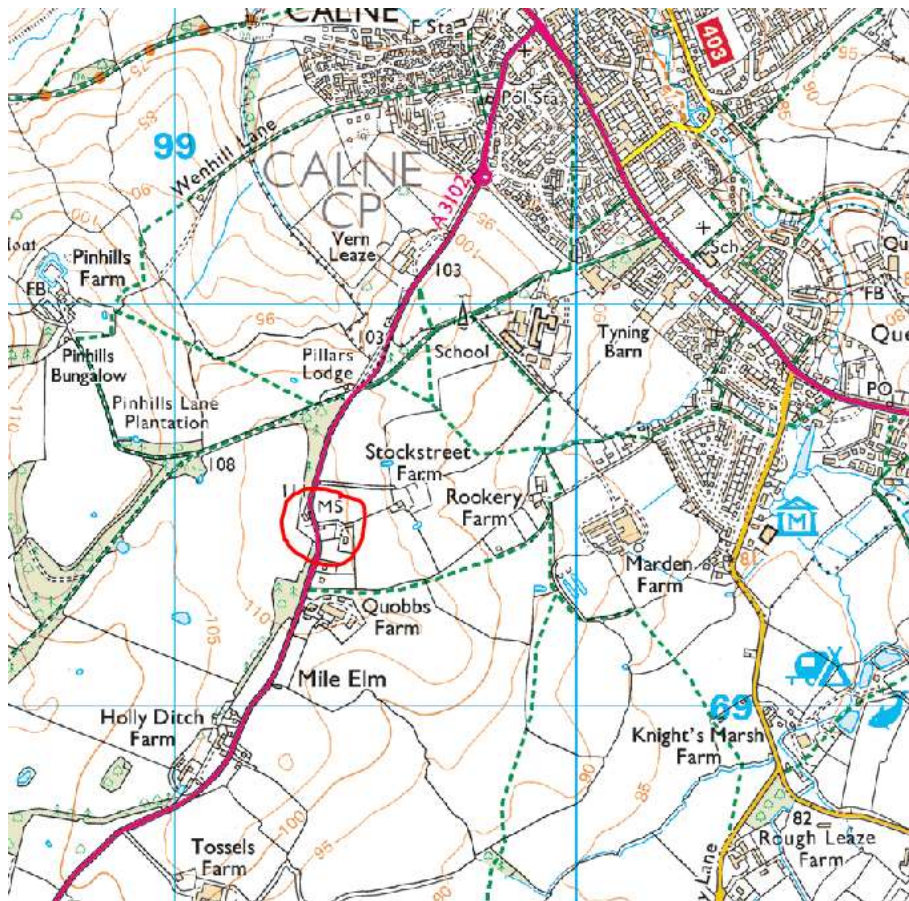
Over the past 15 years or so there has been two separate reviews of the warning signs at this bend and significantly, the road was resurfaced in 2016 after a survey identified that the skid resistance of the road surface no longer met the required standard.

Since the new road surface was applied the number of personal injury collisions recorded here has reduced, with 2 collisions being reported from 1<sup>st</sup> January 2017 and 31<sup>st</sup> March 2022. In both instances the injuries sustained were noted as being “Slight” by the Police collision team.

Residents have requested a fresh review of warning signs at this bend and this request is supported by the Parish Council.

The LHFIG has asked officers to consider additional signing measures.

### Location Plan



### Current Provisions

There are double-bend ahead warning signs with “reduce speed now” supplementary plates positioned on each approach with a SLOW marking painted adjacent to these. The signs are mounted on a grey backing board. They are positioned at an appropriate distance to the bend to give warning of the hazard ahead. There are also chevron signs (single and twin-bladed) facing both directions, although these are randomly positioned, suggesting that others chevron signs might have been placed here but have been “lost” over time. It is also noted that 50 mph repeater signs are positioned on each approach to the bend.

The bend is also covered by a double white centre line system with road studs and these markings are in very good order.

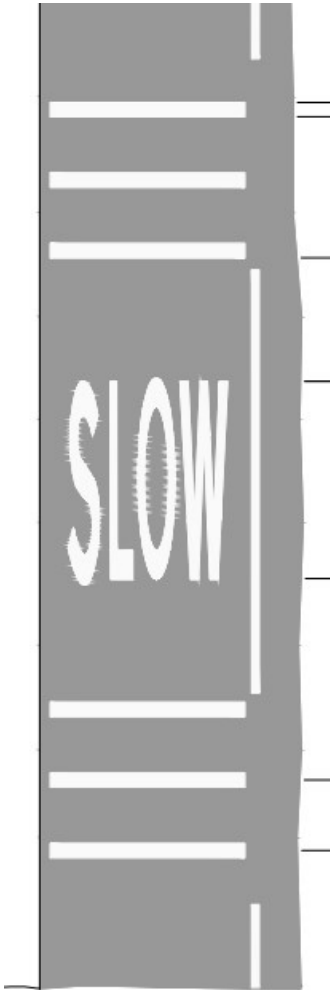
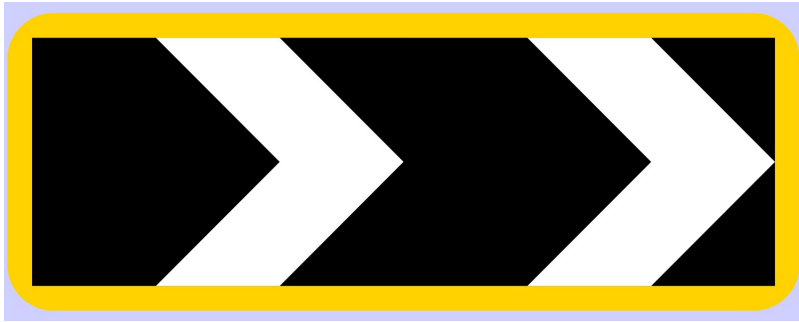
### Recommendations

1. Replace and provide additional chevron signs to include yellow backing boards to raise the conspicuity of these signs.
2. Replace the advance warning signs and swap the “reduce speed now” plates for “Max Speed 30 mph” signs. These could also be mounted on yellow backing boards.
3. Remove the 50 mph repeater signs from this location. The rules on providing repeater signs have been relaxed since this speed limit was introduced and we can omit speed limit repeaters as desired.
4. Provide one additional SLOW marking with transverse bars on each approach to the bend, and refresh and add transverse bars to the existing markings.

All or some of the recommendations can be prioritised.

Examples:





### Potential Costs

A ball-park figure to undertake all elements of this proposal is likely to be in the region of £7,500.

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21/10/22